

BIRCH GREEN

Allotment Gardens

15/01352/FUL

**Venture House, 42-54 London Road,
Staines-upon-Thames, TW18 4HF**



1:1,250

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Planning Committee

13 January 2016



| | | | |
|-----------------------------|--|--------------------|----------------------|
| Application Nos. | 15/01352/FUL | | |
| Site Address | Venture House, 42 – 54 London Road, Staines upon Thames | | |
| Proposal | Removal of surface level car park and erection of a two-storey block of 6 flats (4 no. 1-bed and 2 no. 2-bed) together with associated amenity space | | |
| Applicant | Mr Kunal Rawal | | |
| Ward | Staines | | |
| Call in details | | | |
| Case Officer | Paul Tomson | | |
| Application Dates | Valid: 30.10.2015 | Expiry: 25.12.2015 | Target: Over 8 weeks |
| Executive Summary | <p>This application is seeks the removal of the existing surface level car park at the rear of the site and the erection of a 2-storey building to provide 6 flats. Whilst the site is located within a designated Employment Area, Prior Approval was granted in January 2015 to convert the offices to residential and it is therefore considered that the Council could not object to the loss of the commercial car parking area. The design and scale of the new building is in keeping with the character of the area. The development will have an acceptable relationship with the neighbouring residential properties in New Street and Sidney Road. Satisfactory levels of amenity space and parking will be provided. Furthermore, the scheme is considered acceptable in flood risk terms.</p> | | |
| Recommended Decision | This application is recommended for approval | | |

MAIN REPORT

1. Development Plan

1.1 The following policies in the Council's Core Strategy and Policies DPD 2009 are considered relevant to this proposal:

- LO1 (Flooding)
- EM1 (Employment Development)
- HO4 (Housing Size and Type)
- HO5 (Housing Density)
- EN1 (Design of New Development)
- CC1 (Renewable Energy, Energy Conservation and Sustainable Construction)
- CC3 (Parking Provision)

2. Relevant Planning History

| | | |
|------------|--|------------------------|
| SP/97/0594 | Redevelopment to provide 3-storey office building, with car parking (including basement car parking) and associated works. | Approved 22/04/1998 |
|------------|--|------------------------|

| | | |
|--------------|---|--|
| 14/02058/PDO | Prior Approval Notification for the change of use from offices (Use Class B1a) to residential (Use Class C3) comprising 29 flats. | Prior Approval Given 20/01/2015 |
|--------------|---|--|

(Officer note: the approved plans show 28 units to be provided)

| | | |
|--------------|---|-------------------------|
| 15/00654/FUL | Removal of surface level car park and erection of a two-storey block of 6 flats (4 no. 1-bed and 2 no. 2-bed) together with associated amenity space. | Withdrawn 15/07/2015 |
|--------------|---|-------------------------|

(Officer note: the above application was withdrawn because the Council raised concern regarding the design and scale of the building. The current proposal has been amended to address those concerns)

3. Description of Current Proposal

3.1 The application relates to Venture House, 42 – 54 London Road, which is a 3-storey office building located on the southern side of the road. It has a basement car park, and a surface level car park located towards the rear of the site. The site is located within a designated Employment Area. It is also within an area liable to flood: Zone 2, which has between 1 in 100 year and 1 in 1000 year chance of flooding.

- 3.2 To the east is the adjacent 3-storey office block of Elizabeth House and its associated car park. To the west are the residential properties of 11 – 19 New Street. Further to the west on the other side of New Street is the commercial building of Prism House. To the south are the dwellings of 27 and 29 Sidney Road and their rear gardens.
- 3.3 The proposal involves the removal of the surface level car park and the erection of a 2-storey block of 6 flats. The proposed building will measure 16.3m in depth, 16m in width and up to 8.33m in height. It will be faced in buff coloured brickwork and grey coloured roof tiles. Private and communal amenity space will be provided. The proposed dwelling mix will comprise 4 no. 1-bed and 2 no. 2-bed units. 6 no. designated parking spaces (1 per unit) will be provided: 4 of them will be situated within the existing basement car park, whilst the other 2 will be located adjacent to the basement ramp. The parking provision for the remaining office building will be reduced from 59 to 24.
- 3.4 The scheme has been amended since it was first submitted. In particular, changes have been made to the roof with the introduction of a half-hip to the front elevation. In addition, the proposed site layout plan has been amended with the creation of a turning head to ensure that refuse vehicles can turn around within the site. As a consequence, the number of existing office parking spaces have been reduced from 27 to 24.
- 3.5 It is relevant to note that in January 2015 Prior Approval was granted under the Government's recent relaxation of Permitted Development rights to convert Venture House from Offices (Use Class B1a) to Residential (Use Class C3) to provide 28 flats. At the time of writing, the Prior Approval had not been implemented.
- 3.6 Copies of the proposed site layout, floor plans and elevations are provided as an Appendix.

4. **Consultations**

- 4.1 The following table shows those bodies consulted and their response.

| Consultee | Comment |
|----------------------------------|--|
| County Highway Authority | No objection |
| Environmental Health (Pollution) | No objection subject to conditions relating to contaminated land. No objection on air quality grounds, but recommends that the cycle parking provision on the overall site is increased |
| Environment Agency | No objection subject to conditions |
| Street Scene | No objection to the revised site layout plan showing the provision of a turning head. Has made detailed comments regarding extra signage, removal of existing bollards, dropped kerbs for disabled |

| | |
|----------------------|--|
| | access and other information, which has been forwarded to the applicant. |
| County Archaeologist | No objection subject to a condition |

5. **Public Consultation**

5.1 31 neighbouring properties were notified of the planning application. 2 letters of objection has been received (from 29 and 31 Sidney Road). Issues raised include:

- Loss of privacy/overlooking. Mature trees should be planted in the communal garden to screen views from the flats' windows.
- Loss of light
- Concern about flooding.

6 **Planning Issues**

- Loss of designated Employment Land/Principle
- Housing size and type
- Housing density
- Design and appearance
- Impact on neighbouring residential properties
- Amenity space
- Parking/Loss of existing parking
- Flooding

7 **Planning Considerations**

Loss of designated Employment Land/Principle

7.1 The site is located within a designated Employment Area. Policy EM1 of the Core Strategy and Policies DPD 2009 (CS & P DPD) stipulates that there shall be no loss of employment land or floorspace within an Employment Area. Whilst the proposal will result in the loss of part of the site (i.e. the surface level car park) to residential, it is not considered that the Council could resist the loss of this particular area of designated employment. As mentioned above, in January 2015 Prior Approval was granted for the change of use of the entire site from offices to residential comprising 28 flats. Both the basement and surface level car park was to be used for parking in association with the new flats. Whilst it is likely the applicant would have to re-apply for a revised Prior Approval application with a reduction in the level of car parking (from 59 to 24), the loss of the employment use of the site and the principle of residential development has already been accepted.

Housing Density

7.2 Policy HO5 of the CS & P DPD states that within higher density residential areas, including those characterised by a significant proportion of flats and those containing significant Employment Areas, new development should generally be in the range of 40 – 75 dwellings per hectare (dph). Higher

density development may be acceptable where it is demonstrated that the development complies with Policy EN1 of the CS & P DPD on design, particularly in terms of its compatibility with the character of the area and is in a location that is accessible by non car-based modes of travel.

- 7.3 Whilst the main part of the site is still occupied as offices, the proposed residential density for the southern part of the site where the block of flats is to be sited is 105 dph. However, this does not include the area covered by the proposed designated parking spaces or the access thereto, which would if included, lower the density level. If the existing office building were to be converted to residential and the proposed block of 6 flats implemented, the residential density for the overall site would 170 dph. The proposed building and associated amenity space is considered to be acceptable in design terms and complies with Policy EN1, which is explained in detail in the following paragraphs. Moreover, the site is in a location that is accessible by non car-based modes of travel. A consequence of higher residential density is that a lower parking provision is provided compared to that stipulated in the Council's Parking Standards. However, limited parking provision may still be acceptable in town centre locations with access to non car-based modes of transport. Consequently, the proposed housing density is considered acceptable in this particular location.

Design and Appearance

- 7.4 Policy EN1a of the CS & P DPD states that the Council will require a high standard in the design and layout of new development. Proposals for new development should demonstrate that they will create buildings and places that are attractive with their own distinct identity; they should respect and make a positive contribution to the street scene and the character of the area in which they are situated, paying due regard to the scale, height, proportions, building lines, layout, materials and other characteristics of adjoining buildings and land.
- 7.5 It is considered that the proposed design and appearance is acceptable and is in keeping with the character with the area. The new building will be set back from the London Road frontage and there will be only limited views of it through the gap between the existing office buildings. The proposed building will be faced with buff brickwork and grey coloured tiles to match Venture House. Furthermore, it will have a pitched roof design and be 2-storey in scale, and therefore appear in keeping with the 2-storey residential buildings to the west in New Street. The proposed western elevation is set in from the side boundary by only 0.2 metres, which is less than the normal minimum 1 metre set-in standard stipulated in the Supplementary Planning Document on Residential Extensions and New Residential Development 2011 (SPD). However, the site is not within a traditional residential street scene and there will be no issue of a potential terracing effect. Consequently, the limited set-in from the boundary is considered acceptable and will not otherwise harm the character of the area.

Impact on Neighbouring Residential Properties

- 7.6 Policy EN1b of the CS & P DPD states that new development should achieve a satisfactory relationship to adjoining properties avoiding significant harmful impact in terms of loss of privacy, daylight or sunlight, or overbearing effect due to bulk and proximity or outlook.
- 7.7 The proposal is considered to have an acceptable relationship with neighbouring residential properties. There will be a satisfactory separation distance between the proposed 2-storey western flank elevation and the rear elevations of 11 – 17 New Street. For example, there will be a gap of at least 15.2 metres between the original 2-storey rear elevation of 15 New Street, whilst the distance will be 17.9 metres between the rear elevation of the maisonettes of 11 & 13 New Street. These distances are above the minimum 13.5m “back to flank” separation distance standard stipulated in the SPD. With regard to 27 and 29 Sidney Road, there will be gap of at least 10.7m between the proposed rear elevation and the southern boundary. This is above the minimum 10.5m “back to boundary” standard in the SPD. Consequently, it is considered that there will not be any undue loss of amenity. I also consider the relationship with the office building of Elizabeth House to be acceptable.

Amenity Space

- 7.8 The proposed development comprises a communal garden at the southern end of the site measuring 162 sqm. In addition, 2 small private amenity areas/terraces are to be provided for the ground floor rear flats, each measuring 16 sqm. The combined size of the communal and private amenity spaces/terraces is 194 sqm, which is above the Council minimum amenity space standard of 185 sqm for scheme of this size. Moreover, the garden areas face a southerly direction and therefore have a high amenity value. Accordingly, the proposed amenity space is considered acceptable.

Parking Provision

- 7.9 Policy CC3 (Parking Provision) of the CS & P DPD states that the Council will require appropriate provision to be made for off-street parking in development proposals in accordance with its maximum parking standards.
- 7.10 On 20 September 2011 the Council's Cabinet agreed a 'Position Statement' on how Policy CC3 should now be interpreted in the light of the Government's recent parking policy changes. The effect of this is that the Council will give little weight to the word 'maximum' in relation to residential development when applying Policy CC3 and its residential parking standards will generally be applied as minimum (maximum parking standards continue to be applicable in relation to commercial development). The supporting text to the Parking Standards stipulates a number of exceptional situations where a reduction in parking will only be allowed. One of these situations includes town centre locations where the reduction parking will be assessed against the distance from a "public transport node", frequency of public transport, availability of pedestrian and cycle routes, and the range and quality of facilities supportive of residential development within reasonable walking distance.

- 7.11 The proposed parking provision is 6 spaces (1 space per unit). As mentioned above, these designated spaces are to be located within the existing basement and adjacent to the basement ramp. The normal minimum parking standard for a scheme of this size is 8. The proposed provision is therefore 2 spaces below the standard. The site is located however, within the commercial area of Staines town centre. It is close to the train station which has a frequent and extensive service. It is also within easy walking distance of the shops and services of the town centre (which has the largest range of facilities in the Borough) and the bus station. There are also several bus services in London Road. Consequently, I consider that there is clear justification for a reduction in the level of parking on this particular site.
- 7.12 It is not considered that an objection could be raised to the reduced parking level provided for the remaining office building of Venture House in this town centre location. The remaining number of office parking spaces would be 24. The Council's Maximum Parking Standards (note maximum standards are applied for commercial development, not minimum) stipulate that within Staines town centre employment area provision should not normally exceed 1 car space per 50 sqm gross floorspace. The existing building has 1890 sqm of office floorspace (as stated by the applicant in the previous Prior Approval application) and the proposed commercial car parking provision would be 1 space per 79 sqm. As this is below the maximum standard of 1 space per 50 sqm, the proposed commercial parking level is acceptable.
- 7.13 As mentioned above, if the applicant were to subsequently decide to implement the conversion of the existing office building to residential (and build the proposed development), it is likely they would have to re-apply for a revised Prior Approval application due to the reduced level of parking provision from 59 to 24. This revised parking provision would equate to just under 1 space per unit (0.86). The Council's parking standards would normally stipulate a minimum provision of 39 spaces (a shortfall of 15 spaces). However, as previously stated, the site is in a town centre location where a more flexible approach to parking provision is acceptable. It is not considered that an objection could be raised on parking/highway grounds if this particular scenario were to be carried out on the site. Likewise, the remaining provision of 24 parking spaces for the remaining office building would be acceptable in this location. There is extensive single and double line parking restrictions in the vicinity of the site, which would control and make safe any on-street parking demand.

Flooding

- 7.14 The site is located within Flood Zone 2, which has between 1 in 100 year and 1 in 1000 year chance of flooding. There is no objection in principle to the creation of new residential development in this relatively low risk flood zone. The Environment Agency were consulted and have raised no objection subject to conditions relating to contaminated land and groundwater. In addition conditions are to be imposed to prevent any raising of existing ground levels and requiring the removal of any spoil from construction. Accordingly, the impact on flood risk is considered acceptable.

Other Matters

- 7.15 The development complies with Policy HO4 of the CS & P DPD which requires developments to include at least 80% of their total as one or two bedroom units.
- 7.16 With regard to the representation from the third party, it is not considered that the Council could insist on the planting of mature trees along the rear boundary, particularly as the new building complies with the separation distances standards in the SPD. However, conditions will be imposed requiring details of boundary treatment and landscaping to be submitted for approval, which should ensure that the rear boundary is treated in a satisfactory manner.
- 7.17 The Council's Street Scene has confirmed the size of the bin store is adequate. Furthermore, they are satisfied with the revised site layout plan showing the provision of a turning head. A condition is recommended to ensure that the turning head is implemented and maintained on the site.
- 7.18 The floorspace of the proposed units comply with the minimum standards stipulated in the SPD.
- 7.19 In terms of cycling parking facilities, space for 6 bicycles is provided in a covered area in the new block. This represents 1 cycle parking space per unit which is considered acceptable and accords with the requirement in the Council's Parking Standards.
- 7.20 Accordingly, the application is recommended for approval.

8. Recommendation

8.1 GRANT subject to the following conditions:-

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason:- This condition is required by Section 91 of the Town and Country Planning Act, 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. The development hereby permitted shall be carried out in accordance with the following approved plans and drawings:

A00-(00) received 12 October 2015.

A00-07 received 29 October 2015.

A00-02 Rev. D; /03 Rev. D; /04 Rev. E; /05 Rev. E; /06 Rev. E received 01 December 2015

A00-01 Rev. C received 07 December 2015

Reason:- For the avoidance of doubt and in the interest of proper planning

3. Before any work on the development hereby permitted is first commenced details of the materials and detailing to be used for the external surfaces of the buildings be submitted to and approved by the Local Planning Authority.

Reason:- To ensure that the proposed development does not prejudice the appearance of the development and the visual amenities and character of the locality, in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

4. No development shall take place until:-
 - (a) A comprehensive desk-top study, carried out to identify and evaluate all potential sources and impacts of land and/or groundwater contamination relevant to the site, has been submitted to and approved in writing by the Local Planning Authority.
 - (b) Where any such potential sources and impacts have been identified, a site investigation has been carried out to fully characterise the nature and extent of any land and/or groundwater contamination and its implications. The site investigation shall not be commenced until the extent and methodology of the site investigation have been agreed in writing with the Local Planning Authority.
 - (c) A written method statement for the remediation of land and/or groundwater contamination affecting the site shall be agreed in writing with the Local Planning Authority prior to the commencement of remediation. The method statement shall include an implementation timetable and monitoring proposals, and a remediation verification methodology.

The site shall be remediated in accordance with the approved method statement, with no deviation from the statement without the express written agreement of the Local Planning Authority.

Reason:-

- (a) To protect the amenities of future residents and the environment from the effects of potentially harmful substances.

NOTE

The requirements of the above Condition must be carried out in accordance with current best practice. The applicant is therefore advised to contact Spelthorne's Pollution Control team on 01784 446251 for further advice and information before any work commences. An information sheet entitled "Land Affected By Contamination: Guidance to Help Developers Meet Planning Requirements" proving guidance can also be downloaded from Spelthorne's website at www.spelthorne.gov.uk.

In accordance with policies SP6 and EN15 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

5. Prior to the first use or occupation of the development, and on completion of the agreed contamination remediation works, a validation report that demonstrates the effectiveness of the remediation carried out shall be submitted to and agreed in writing by the Local Planning Authority.

Reason:- To protect the amenities of future residents and the environment from the effects of potentially harmful substances.

6. No development shall commence until a report has been submitted to and agreed by the Local Planning Authority which includes details and drawings demonstrating how 10% of the energy requirements generated by the development as a whole will be achieved utilising renewable energy methods and showing in detail the estimated sizing of each of the contributing technologies to the overall percentage. The detailed report shall identify how renewable energy, passive energy and efficiency measures will be generated and utilised for each of the proposed buildings to meet collectively the requirement for the scheme. The agreed measures shall be implemented with the construction of each building and thereafter retained and maintained to the satisfaction of the Local Planning Authority unless otherwise agreed in writing.

Reason:- To ensure that the development is sustainable and complies with Policy SP7 and CC1 of the Spelthorne Development Plan Core Strategy and Policies DPD.

7. No development shall take place until there has been submitted to and approved in writing by the Local Planning Authority a plan indicating the positions, design, materials and type of boundary treatment to be erected. The details shall include plans and elevations of the proposed sliding entrance gates. The boundary treatment and gates shall be completed before the buildings are occupied. Development shall be carried out in accordance with the approved details and maintained as approved.

Reason:- To ensure that the proposed development does not prejudice the enjoyment by neighbouring occupiers of their properties and the appearance of the locality, in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

8. No development shall take place until full details of both soft and hard landscape works have been submitted to and approved in writing by the Local Planning Authority and these works shall be carried out as approved. The trees, shrubs and other associated proposals shall be planted on the site within a period of 12 months from the date on which development hereby permitted is first commenced, or such longer period as may be approved by the Local Planning Authority, and that the planting so provided shall be maintained as approved for a period

of 5 years, such maintenance to include the replacement in the current or next planting season whichever is the sooner, of any trees or shrubs that may die, are removed or become seriously damaged or diseased, with others of similar size and species, unless the Local Planning Authority gives written permission to any variation.

Reason:- To minimise the loss of visual amenity occasioned by the development and to enhance the proposed development.

9. Before the first occupation of any part of the development, a landscape management plan including long-term design objectives, management responsibilities and maintenance schedules for all landscaped areas shall be submitted to and approved in writing by the Local Planning Authority. The landscape management plan shall be carried out as approved.

Reason:- To minimise the loss of visual amenity occasioned by the development and to enhance the proposed development.

10. The 6 no. allocated car parking spaces and access/turning area thereto, as shown on approved drawing no. A00-07 received 29/10/2015, shall be permanently maintained for their designated purpose.

Reason:- The above condition is required in order that the development should not prejudice highway safety nor cause inconvenience to other highway users in accordance with policy CC2 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

11. No development shall take place until a Method of Construction Statement, to include details of:
 - (a) parking for vehicles of site personnel, operatives and visitors
 - (b) loading and unloading of plant and materials
 - (c) storage of plant and materials
 - (d) measures to keep the highway clear from deposit of site materials
 - (e) measures to minimise dust during constructionhas been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction period.

Reason:- In the interest of highway safety and to minimise dust pollution during construction.

12. No new development shall be occupied until space has been laid out in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority to provide secure and covered cycle parking for both the residential development hereby permitted and the remaining development at Venture House, and thereafter shall be maintained and approved.

Reason:- The above condition is required in recognition of Section 4 (Promoting Sustainable Transport) of the NPPF

13. Before any development commences, details including a technical specification of all proposed external lighting shall be submitted to and approved in writing by the Local Planning Authority. The external lighting on the site shall at all times accord with the approved details.

Reason:- To safeguard the amenity of neighbouring residential properties and in the interest of security.

14. That within 3 months of the commencement of any part of the development permitted, or such longer period as may be approved by the Local Planning Authority, facilities shall be provided within the curtilage of the site for the storage of refuse and waste materials in accordance with the details hereby approved, and thereafter shall be maintained as approved.

Reason:- To ensure that the proposed development does not prejudice the enjoyment by neighbouring occupiers of their properties and the appearance of the locality, in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

15. No development shall take place until the applicant has secured the implementation of a programme of archaeological work in accordance with a Written Scheme of Investigation which has been submitted by the applicant and approved by the Local Planning Authority.

Reason:- The site is of high archaeological potential and it is important that the archaeological information should be preserved as a record before it is destroyed by the development.

16. No infiltration of surface water drainage into the ground within the development site is permitted other than with the express written consent of the local planning authority, which may be given for those parts of the site where it has been demonstrated that there is no resultant unacceptable risk to controlled waters. The development shall be carried out in accordance with the approval details.

Reason:- To prevent pollution of the water environment due to possible migration of contaminants from within the contaminated land. National Planning Policy Framework paragraph 109 states that the planning system should contribute to and enhance the natural and local environment by preventing both new and existing development from contributing to or being put at unacceptable risk from, or being adversely affected by unacceptable levels water pollution. This condition is also sought in accordance with part (d) of policy EN9 of Spelthorne Borough Council's local plan adopted in 2009.

17. Piling or any other foundation designs using penetrative methods shall not be permitted other than with the express written consent of the local

planning authority, which may be given for those parts of the site where it has been demonstrated that there is no resultant unacceptable risk to groundwater. The development shall be carried out in accordance with the approved details.

Reason:- Piling or any other foundation designs using penetrative methods can result in risks to potable supplies from, for example, pollution / turbidity, risk of mobilising contamination, drilling through different aquifers and creating preferential pathways. Thus it should be demonstrated that any proposed piling will not result in contamination of groundwater. The National Planning Policy Framework paragraph 109 states that the planning system should contribute to and enhance the natural and local environment by preventing both new and existing development from contributing to or being put at unacceptable risk from, or being adversely affected by unacceptable levels water pollution.

18. There shall be no raising of existing ground levels on the site within the area liable to flood, other than in accordance with the approved details.

Reason: -To prevent the increased risk of flooding due to impedance of flood flows and reduction in flood storage capacity in accordance with policies SP1, SP7 and LO1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

19. All spoil and building materials stored on site before and during construction shall be removed from the area of land liable to flood upon completion

Reason:- To prevent the increased risk of flooding due to impedance of flood flows and reduction of flood storage capacity in accordance with policies SP1, SP7 and LO1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

20. Prior to the commencement of development, details of the works to be carried out on the site to create the turning head and revised disabled parking space shall be submitted to and approved in writing by the Local Planning Authority. The details shall include amendments to the footway and measures to prevent cars parking in the approved turning head. The approved works and measures shall be carried out in accordance with the agreed details before the occupation of the new building, and thereafter maintained.

Reason: To ensure that a refuse vehicle and other service vehicles can enter and exit the site in forward gear, in the interest of highway safety.

Informatives

1. Please note that this application is subject to the payment of Community Infrastructure Levy (CIL). Full details of the charge, how it has been calculated and what happens next are set out in the CIL Liability Notice which will be sent separately.

If you have not already done so an Assumption of Liability notice should be sent to the Council as soon as possible and before the commencement of development.

Further information on CIL and the stages which need to be followed is available on the Council's website. www.spelthorne.go.uk/CIL.

2. The applicant is advised that the site to which this planning permission relates is located on or near land that may contain harmful substances. Under Part C of the Building Regulations you will be required to consider this when designing the foundations of the development.





The applicant is advised to contact Spelthorne's Pollution Control team on 01784 446251 for further advice and information before any work commences.

Article 2(3) Development Management Procedure (Amendment) Order 2012

In assessing this application, officers have worked with the applicant in a positive and proactive manner consistent with the requirements of paragraphs 186-187 of the NPPF. This included the following:-

- a) Provided pre-application advice to seek to resolve problems before the application was submitted and to foster the delivery of sustainable development.
- b) Provided feedback through the validation process including information on the website, to correct identified problems to ensure that the application was correct and could be registered;
- c) Have suggested/accepted/negotiated amendments to the scheme to resolve identified problems with the proposal and to seek to foster sustainable development.
- d) Have proactively communicated with the applicant through the process to advise progress, timescales or recommendation.

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 It is the contractors responsibility to ensure full compliance with the Building Regulations.
 Do not scale from this drawing, use figured dimensions only.
 It is the contractors responsibility to check and verify all dimensions on site.
 Any discrepancies to be reported immediately.
 IF IN DOUBT ASK.
 Materials not in conformity with relevant British or European Standards/Codes of practice or materials known to be deleterious to health & safety must not be used or specified on this project.
 Materials known to contain asbestos contaminated materials (ACM's) in the manufacture or installation process have not been specified and must not be used on this project.

- KEY**
- Timber Fence 
 - Site Boundary 
 - Lighting Path 
 - Security Gate 

NOTES

The boundary to the front of the site will be treated with a 450mm high timber knee rail fence. The boundary to the sides and rear of the site will be treated via the use of a timber closeboarded fence, min 2.0m high in compliance with Secure by Design.

The front of the site will be finished with either hardscape or grass and shrub planting.
 Controlled access to the rear of the property will be achieved by the installation of a gate at the side with access control to the apartments accessed from the rear.

Flats facing onto the rear will be provided with a private area. This area to be defined by use of a low level timber fence. The rear of the site will have an area of communal garden for enjoyment by each of the flats. This area will be primarily finished with grass and shrub planting.

Path of lighting is indicative, design TBC, spec to be eco friendly and low wattage.

All doors and windows to be PAS 24 rated.

Nb.
 Site plan currently based on OS data. A further topographical survey required to determine site boundary.

| C | 07.12.15 | Parking amended to accommodate refuse vehicle path. | AM | DRJ |
|----|----------|---|----|-----|
| B | 02.10.15 | Drawing number amended. | AM | DRJ |
| A | 09.09.15 | Site note expanded | SH | RH |
| No | Date | Description | By | Ckd |

CLIENT



PROJECT
 Proposed Resi to rear of 42
 London Rd, Staines Upon
 Thames

TITLE
 Proposed Site Plan

DRAWING STATUS

For Information

| | | | |
|-------|-------------------|---------|----|
| DRAWN | SH | CHECKED | RH |
| SCALE | As indicated @ A1 | | |
| DATE | 09/08/15 | | |

wcec architects
 South Building, Staple Inn Buildings
 London, WC1V 7PZ

t:020 3388 0019, e: email@wcec.co.uk, www.wcec.co.uk

| | | |
|---------|-------------|-----|
| JOB NO. | DRAWING NO. | REV |
| 13-521 | A00-01 | C |



2 no. parking spaces removed and rear of bay set back 1.5 metres to accommodate swept path (including overhang) of refuse vehicle

Venture House

KEEP CLEAR
 VISITOR PARKING

Flat 1
 2b/3p
 67 m²

Flat 3
 1b/2p
 52 m²

Flat 2
 1b/2p
 53 m²

Flat 3
 Amenity

Flat 2
 Amenity

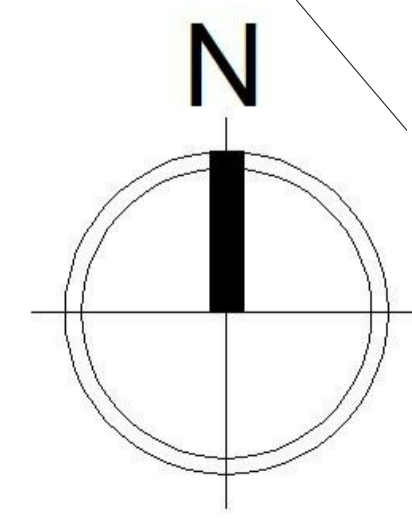
Shared Amenity

Distance
 7.5m

Distance
 1m

Distance
 10.7m

Site Plan
 1 : 100



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Life Time Homes Legend

Criterion 4 – Entrances

- All entrances should:
 a) Be illuminated
 b) Have level access over the threshold; and
 c) Have effective clear opening widths and nibs as specified below.
 In addition, main entrances should also:
 d) Have adequate weather protection*
 e) Have a level external landing.*

Criterion 6 – Internal doorways and hallways

Movement in hallways and through doorways should be as convenient to the widest range of people, including those using mobility aids or wheelchairs, and those moving furniture or other objects.

As a general principle, narrower hallways and landings will need wider doorways in their side walls.

The width of doorways and hallways should conform to the specification below.

Criterion 7 – Circulation Space

There should be space for turning a wheelchair in dining areas and living rooms and basic circulation space for wheelchair users elsewhere.

Criterion 8 – Entrance level living space

A living room / living space should be provided on the entrance level of every dwelling

Criterion 13 – Potential for fitting of hoists and bedroom / bathroom relationship

Structure above a main bedroom and bathroom ceilings should be capable of supporting ceiling hoists and the design should provide a reasonable route between this bedroom and the bathroom.

Criterion 14 – Bathrooms

An accessible bathroom, providing ease of access in accordance with the specification below, should be provided in every dwelling on the same storey as a main bedroom.

Criterion 15 – Glazing and window handle heights

Windows in the principal living space (typically the living room), should allow people to see out when seated. In addition, at least one opening light in each habitable room should be approachable and usable by a wide range of people – including those with restricted movement and reach

| No | Date | Description | By | Ckd |
|----|----------|--|----|-----|
| D | 01.12.15 | Roof updated to introduce hip to North Elevation. PV moved to South Elevation. | AM | DRJ |
| C | 09.09.15 | Flat 4 cupboard door repositioned | SH | RH |
| B | 08.09.15 | Drawing title amended | SH | RH |
| A | 02.09.15 | Alterations to internal layouts with the inclusion of Lifetime Homes | SH | RH |

CLIENT



PROJECT
 Proposed Resi to rear of 42
 London Rd, Staines Upon
 Thames
 TITLE
 Proposed GA Floor Plans

DRAWING STATUS
 For Information

DRAWN AM CHECKED DRJ

SCALE As Indicated @ A1

DATE 03/12/15



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| JOB NO. | DRAWING NO. | REV |
|---------|-------------|-----|
| 13-521 | A00-02 | D |



00 Ground Floor Plan
 1 : 50

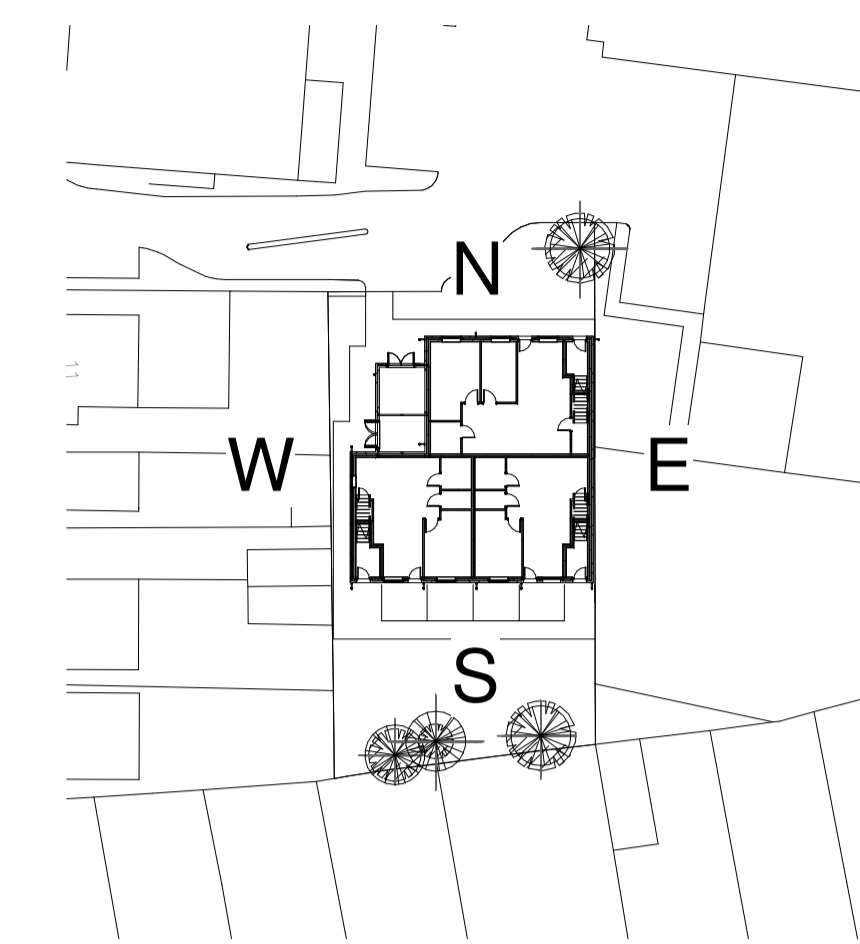


01 First Floor Plan
 1 : 50

| Room Schedule | | | |
|---------------|-----------|-------------------|--------------|
| Number | Occupancy | Area | Area (Sq Ft) |
| Flat 1 | 2b/3p | 67 m ² | 722 SF |
| Flat 2 | 1b/2p | 53 m ² | 567 SF |
| Flat 3 | 1b/2p | 52 m ² | 557 SF |
| Flat 4 | 2b/3p | 71 m ² | 762 SF |
| Flat 5 | 1b/2p | 53 m ² | 566 SF |
| Flat 6 | 1b/2p | 51 m ² | 553 SF |

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Roof Ridge 8330
 Roof 4650
 1st FL 3150
 GFL 150

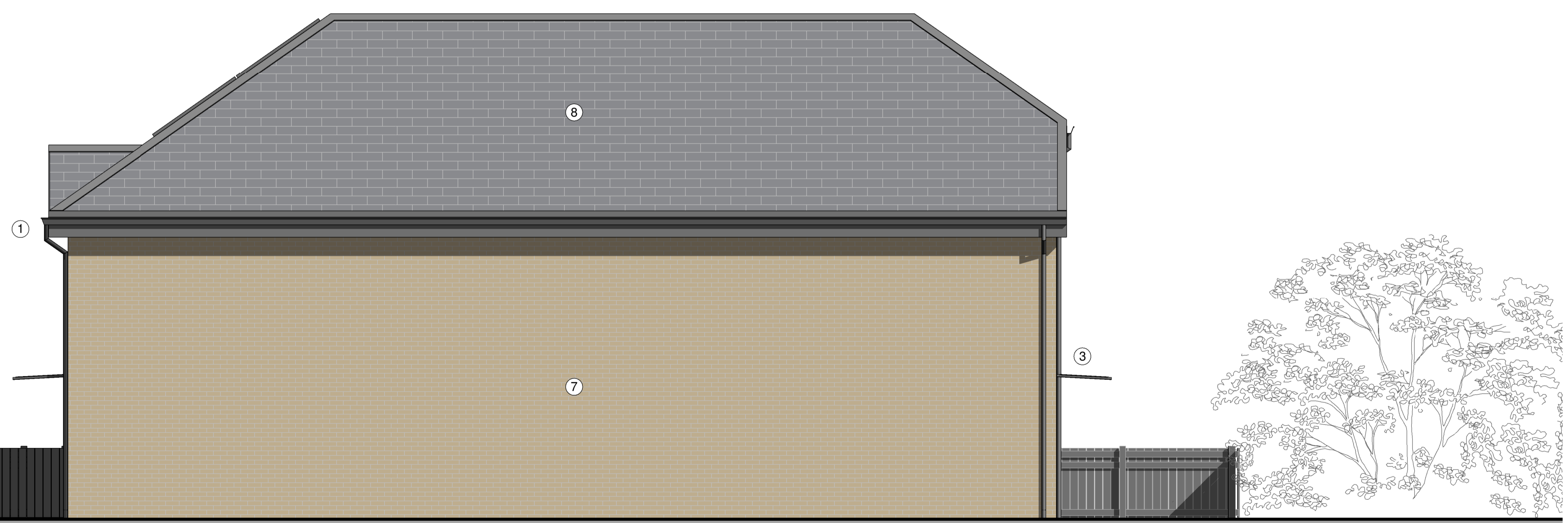


KEY

- 1 - Aluminium Rain water Goods
- 2 - Aluminium Coping Trim
- 3 - Glass/ Polycarbonate Canopy
- 4 - PV Array
- 5 - Obscure Glass Window Fixed Light
- 6- Timber Effect Cladding
- 7- Buff Brickwork to Match Venture House
- 8- Grey Fibre Cement Tile

South Elevation
 1 : 50

Roof Ridge 8330
 Roof 4650
 1st FL 3150
 GFL 150



East Elevation
 1 : 50

| No | Date | Description | By | Ckd |
|----|----------|--|----|-----|
| E | 01.12.15 | Roof updated to introduce hip to North Elevation. PV moved to South Elevation. | AM | DRJ |
| D | 09.09.15 | Timber fence and finishes added to elevations | SH | RH |
| C | 08.09.15 | Alterations to roof | SH | RH |
| B | 02.09.15 | Alterations to Fenestration | SH | RH |
| A | 24.04.15 | Levels raised 150mm in line with flood prevention. | AM | DRJ |

CLIENT



PROJECT
 Proposed Resi to rear of 42
 London Rd, Staines Upon
 Thames
 TITLE
 Proposed GA Elevations

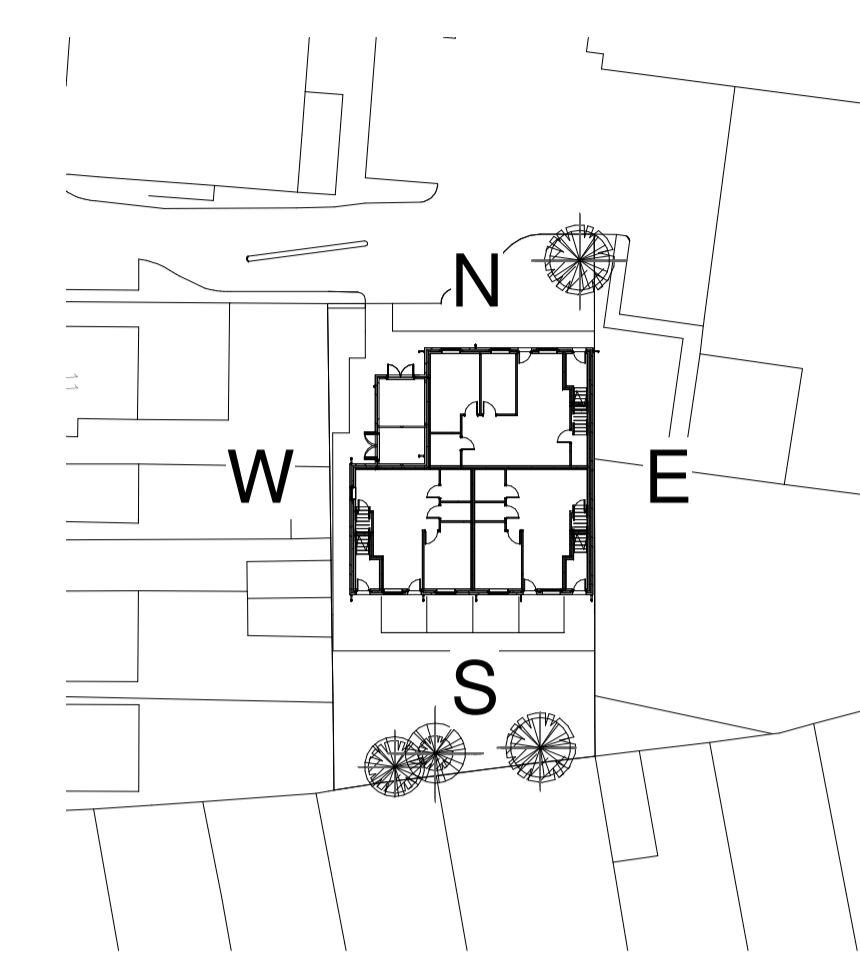
DRAWING STATUS
 For Information
 DRAWN AM CHECKED DRJ
 SCALE As indicated @ A1
 DATE 03/12/15

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| 13-521 | A00-04 | E |

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Roof Ridge 8330
 Roof 4650
 1st FL 3150
 GFL 150



- KEY**
- 1 - Aluminium Rain water Goods
 - 2 - Aluminium Coping Trim
 - 3 - Glass/ Polycarbonate Canopy
 - 4 - PV Array
 - 5 - Obscure Glass Window Fixed Light
 - 6 - Timber Effect Cladding
 - 7 - Buff Brickwork to Match Venture House
 - 8 - Grey Fibre Cement Tile

North Elevation
 1 : 50

Roof Ridge 8330
 Roof 4650
 1st FL 3150
 GFL 150



West Elevation
 1 : 50

| No | Date | Description | By | Ckd |
|----|----------|--|----|-----|
| E | 01.12.15 | Roof updated to introduce hip to North Elevation. PV moved to South Elevation. | AM | DRJ |
| D | 09.09.15 | Timber fence and finishes added to elevations | SH | RH |
| C | 08.09.15 | Alterations to roof | SH | RH |
| B | 02.09.15 | Alterations to Fenestration | SH | RH |
| A | 24.04.15 | Levels raised 150mm in line with flood prevention. | AM | DRJ |

CLIENT
ASHBURY & BLOOM
 SETTING TRENDS IN PROPERTY DEVELOPMENT

PROJECT
 Proposed Resi to rear of 42
 London Rd, Staines Upon
 Thames
 TITLE
 Proposed GA Elevations 2

DRAWING STATUS
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 SCALE As indicated @ A1
 DATE 03/13/15

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